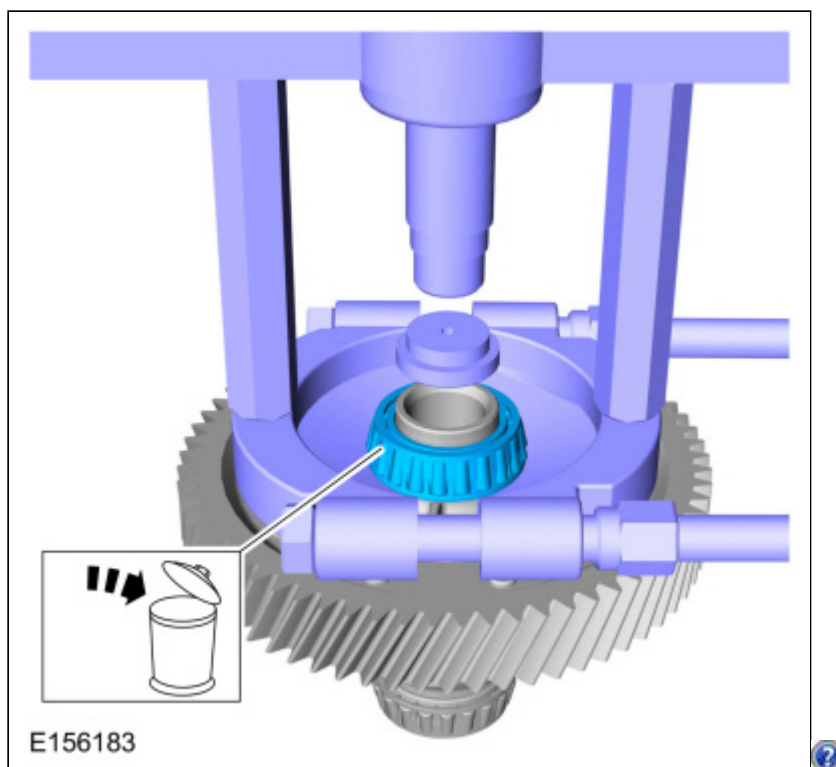


Differential

DISASSEMBLY

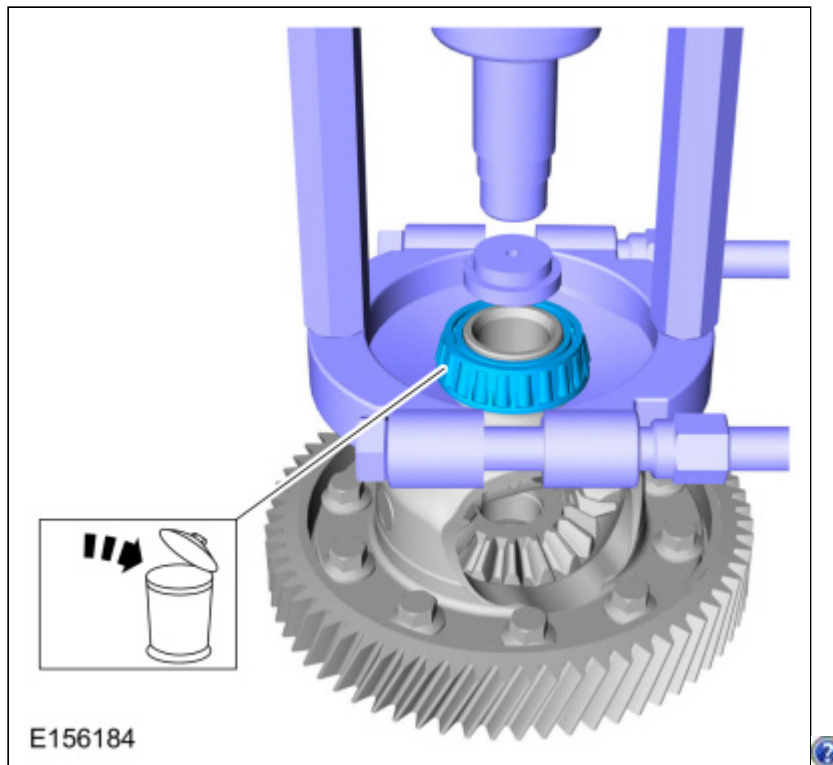
NOTICE: Use vise jaw protectors.

1. Clamp the differential in a vise.
2. *General Equipment* : Bearing Separator
General Equipment : Puller



I cut the roller cages off and removed the rollers prior to doing this, as they just got in the way. Heavy nippers will make easy work of the cages. A bearing separator will be fine here on this side.

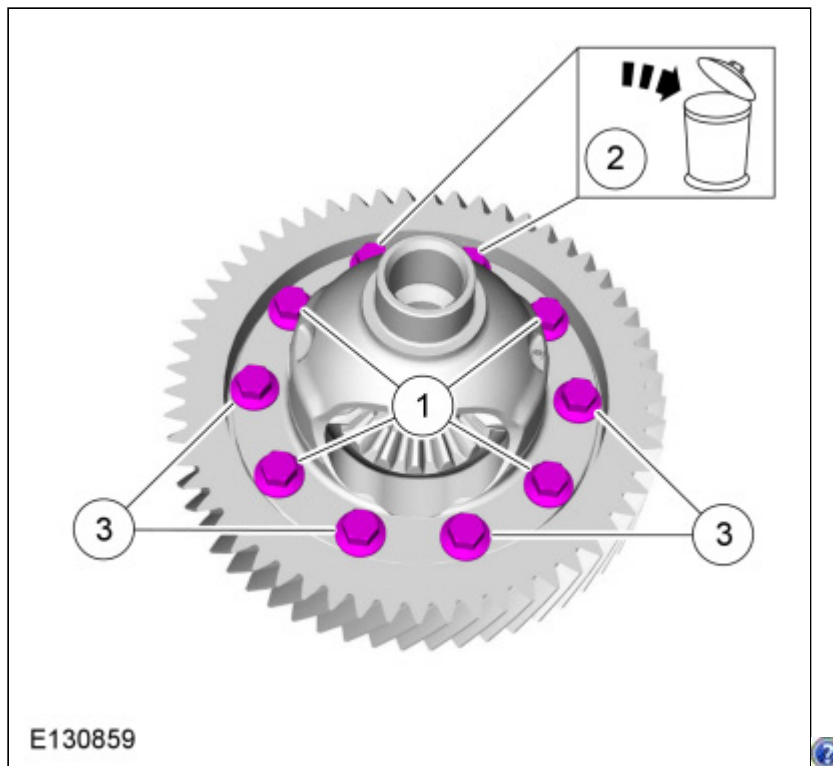
3. *General Equipment* : Bearing Separator
General Equipment : Puller



This side was a lot more challenging. What I ended up doing was removing the cage and rollers as with the other side. BUT I could not remove the race with my separator. What I did was use my Dremel to CAREFULLY cut a long, skinny notch on each side to slip the bearing separator into. DO NOT CUT THROUGH THE RACE INTO THE SHAFT! But do make the notch big enough for the puller to "grab" onto. After doing so, mine came right off.

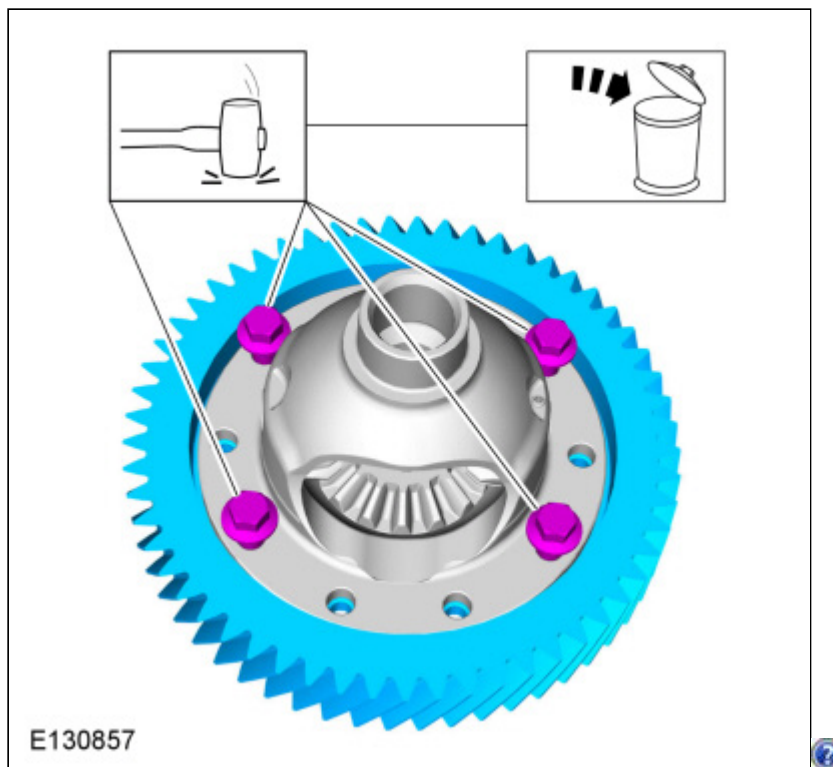
4. Remove the following items:

1. Loosen : Four turn(s)
- 2.
3. Save four bolts for reassembly the ring gear.



Skip this step unless you are replacing the ring gear.

5.



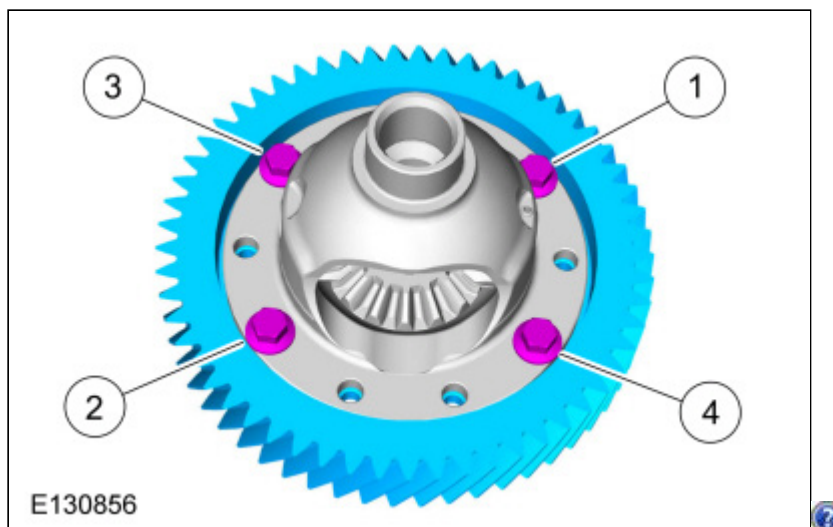
Skip this step unless you are replacing the ring gear.

ASSEMBLY

NOTICE: Use vise jaw protectors.

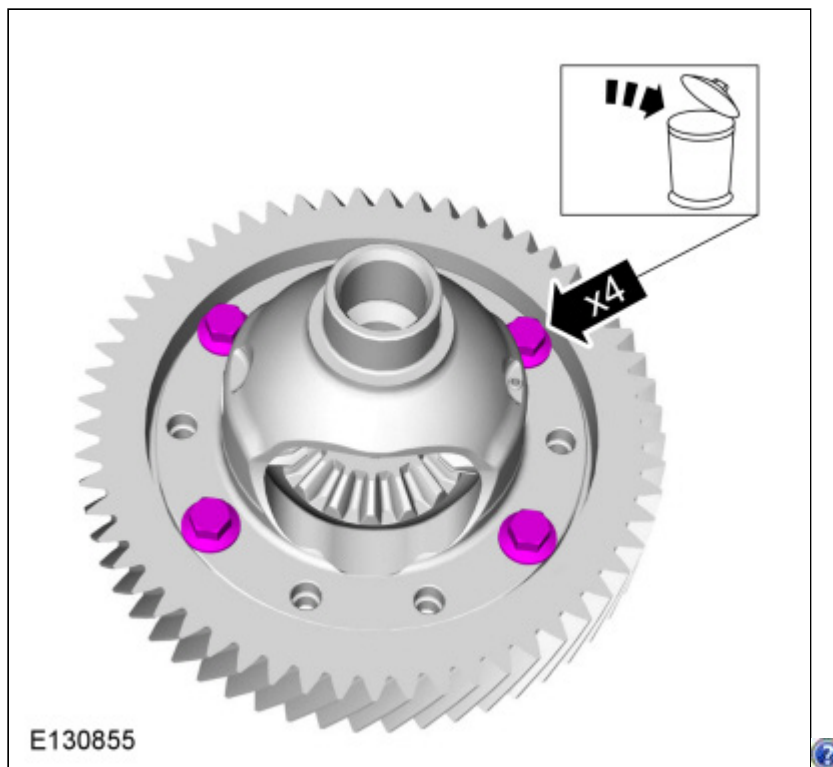
1. Clamp the differential in a vise.
2. **NOTE:** Make sure that the mating faces are clean and free of foreign material.
 - Install the four bolts saved during disassembly.
 - Tighten each bolt 1 turn at a time in the sequence shown.

Torque : 50 Nm



Skip this step unless you are replacing the ring gear.

3.



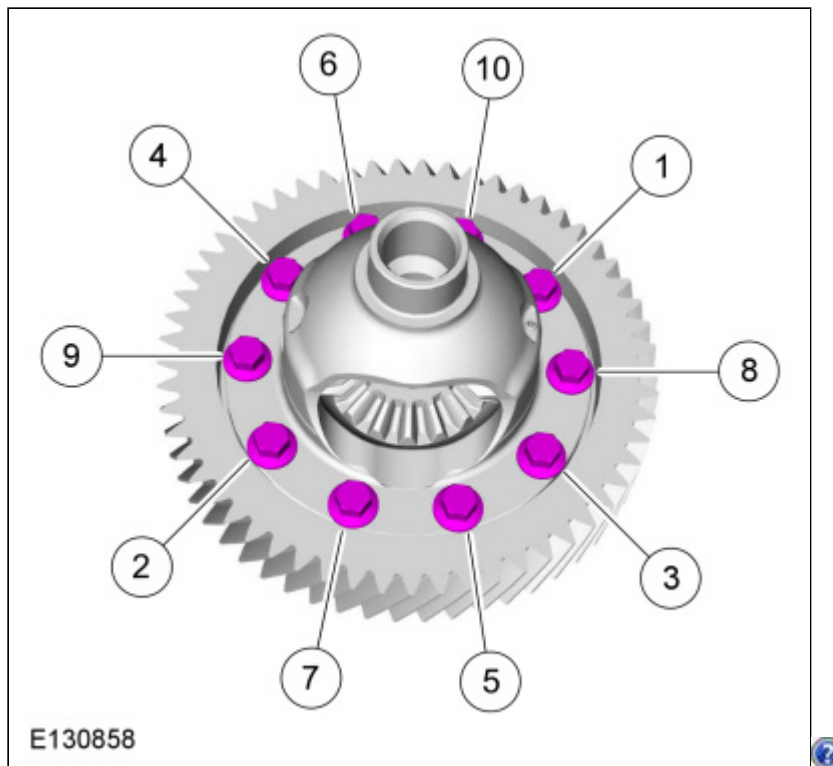
Skip this step unless you are replacing the ring gear.

4. Tighten each bolt 1 turn at a time.

Torque :

Stage 1: 50 Nm

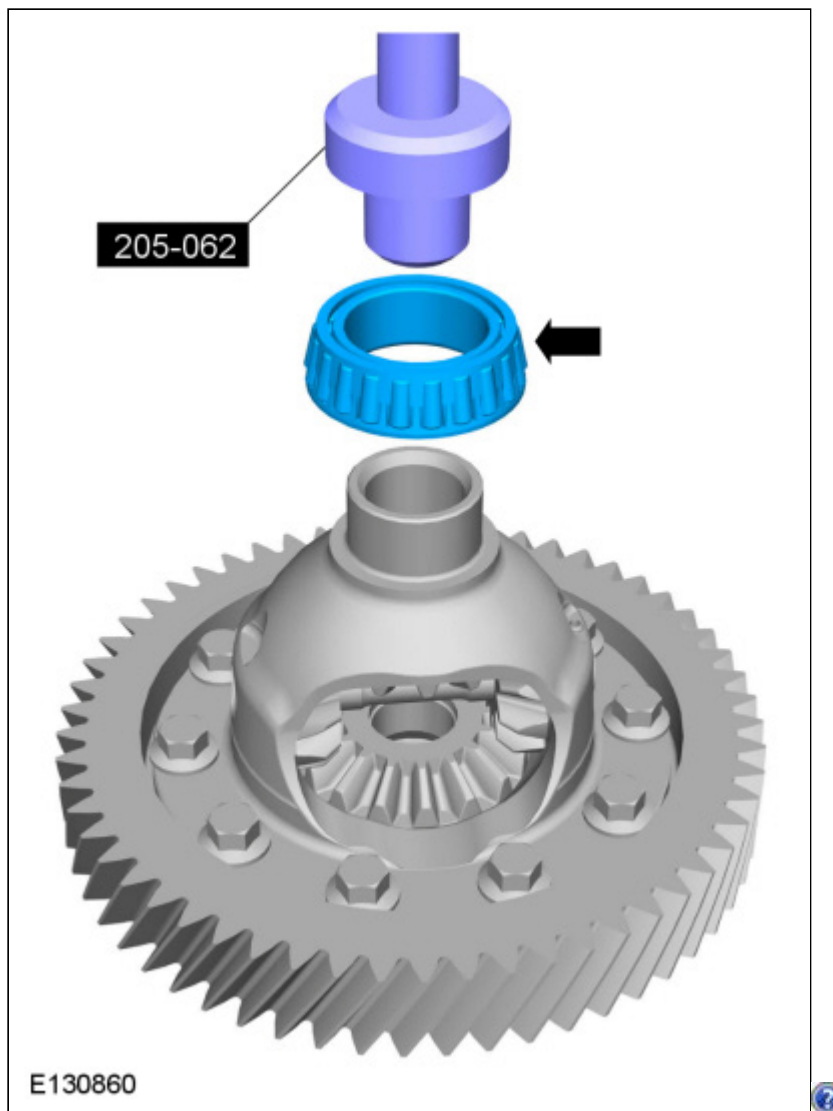
Stage 2: 90°



Skip this step unless you are replacing the ring gear.

5. The roller bearing must not be lubricated.

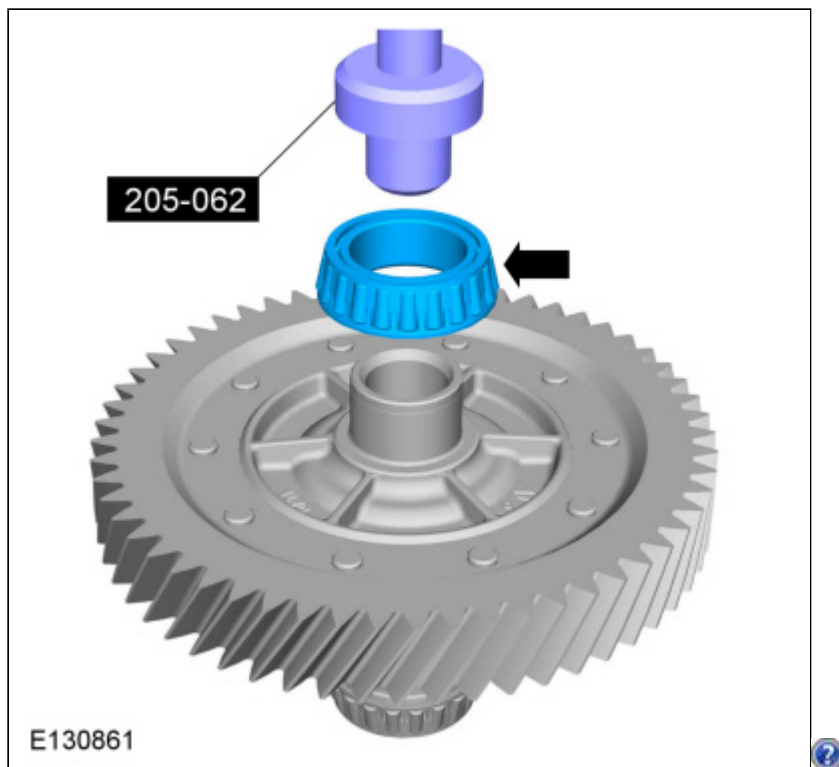
Special Tool(s) : 205-062 Installer, Differential Bearing



The special tool is not necessary, but DO be sure to ONLY press on the race and NOT the cage/rollers!! This is critical!! I used a large socket and some flat steel bar stock.

And remember, do not lubricate the bearing!

6. The roller bearing must not be lubricated.
Special Tool(s) : 205-062 Installer, Differential Bearing



Same as prevoius step.